

**MARYLAND HISTORICAL TRUST
DETERMINATION OF ELIGIBILITY FORM**

NR Eligible: yes ☐ no ☒

Property Name: SHA Bridge No. 0307100, MD 128 over CSX Railroad Inventory Number: BA-2070
Address: Butler Road (MD 128) on "Horses and Hounds" Scenic Byway Historic district: ☐ yes ☒ no
City: Glyndon Zip Code: 21071 County: Baltimore County
USGS Quadrangle(s): Reisterstown
Property Owner: State Highway Administration Tax Account ID Number: _____
Tax Map Parcel Number(s): _____ Tax Map Number: _____
Project: Reevaluation of Highway Bridges Statewide Agency: FHWA/MD SHA
Agency Prepared By: KCI Technologies, Inc.
Preparer's Name: Alison Ross Date Prepared: 10/16/2009
Documentation is presented in: Project Review and Compliance Files
Preparer's Eligibility Recommendation: ☒ Eligibility recommended ☐ Eligibility not recommended
Criteria: ☐ A ☐ B ☒ C ☐ D Considerations: ☐ A ☐ B ☐ C ☐ D ☐ E ☐ F ☐ G
Complete if the property is a contributing or non-contributing resource to a NR district/property
Name of the District/Property: Glyndon Historic District
Inventory Number: BA-2210 Eligible: ☒ yes Listed: ☐ yes
Site visit by MHT Staff ☐ yes ☒ no Name: _____ Date: _____

Description of Property and Justification: *(Please attach map and photo)*

Bridge No. 0307100 (MIHP No. BA-2070) is a 5-span, 2-lane, concrete-slab bridge that carries MD 128 over the CSX Railroad (formerly the Western Maryland Railroad) in Glyndon, Baltimore County, Maryland. Constructed in 1947, the bridge consists of a concrete slab supported by 2 concrete abutments with stone veneer and 4 concrete piers with stone veneer and concrete caps. Bridge No. 0307100 is located within the Glyndon Historic District (MIHP BA-2210). It has not been singled out as a contributing resource to the historic district; its construction date postdates the period of significance of the district, which is the 19th century. The bridge is on Maryland's "Horses and Hounds" Scenic Byway. The bridge's 2006 Average Daily Traffic (ADT) count is 16,411, and the 2026 future ADT count is 18,922. The function class is Urban Other Principal Arterial.

Background

The first evaluation of SHA Bridge No. 0307100 was completed in 1995, for which a Maryland Inventory of Historic Properties (MIHP) form was completed. The Interagency Historic Highway Bridge Inventory Committee (HHBIC) considered the MIHP form in 1996 and subsequently determined Bridge No. 0307100 to be eligible for the National Register of Historic Places (NRHP) under Criterion C as a significant example of concrete slab bridge construction. The MIHP form states that the structure has a high

MARYLAND HISTORICAL TRUST REVIEW

Eligibility recommended ☐ Eligibility not recommended ☐
Criteria: ☐ A ☐ B ☐ C ☐ D Considerations: ☐ A ☐ B ☐ C ☐ D ☐ E ☐ F ☐ G

MHT Comments: *Information purposes only - Bridge remains eligible*

Jim Tullman
Reviewer, Office of Preservation Services

5/14/2010
Date

Reviewer, National Register Program

Date

Bridge No. 0307100 retains its integrity of setting, location, feeling, association, design, materials, and workmanship. The repairs to the stone veneer on all of the CDEs have been completed in a sympathetic fashion and have not diminished their integrity. Bridge No. 0307100 is located within the Glyndon Historic District (MIHP BA-2210). It has not been singled out as a contributing resource to the historic district; its construction date postdates the period of significance of the district, which is the 19th century. Bridge No. 0307100 is recommended individually eligible for listing in the NRHP under Criterion C for its engineering and unique architectural character and because it is a significant example of the work of the Maryland engineering firm, George W.M. Stephens and Associates, and the Baltimore architecture firm, Palmer and Lamdin. The bridge exhibits an unusual combination of concrete and stone masonry construction not often found on Baltimore County bridges. Additional research indicates that the bridge is not associated with known events of local, regional, or national significance (Criterion A), or known persons of local, regional, or national significance (Criterion B). Criterion D was not evaluated as part of the historic standing structures studies for this project.

MARYLAND HISTORICAL TRUST REVIEW

Eligibility recommended _____ Eligibility not recommended _____

Criteria: ___A ___B ___C ___D Considerations: ___A ___B ___C ___D ___E ___F ___G

MHT Comments:

Reviewer, Office of Preservation Services_____
Date_____
Reviewer, National Register Program_____
Date

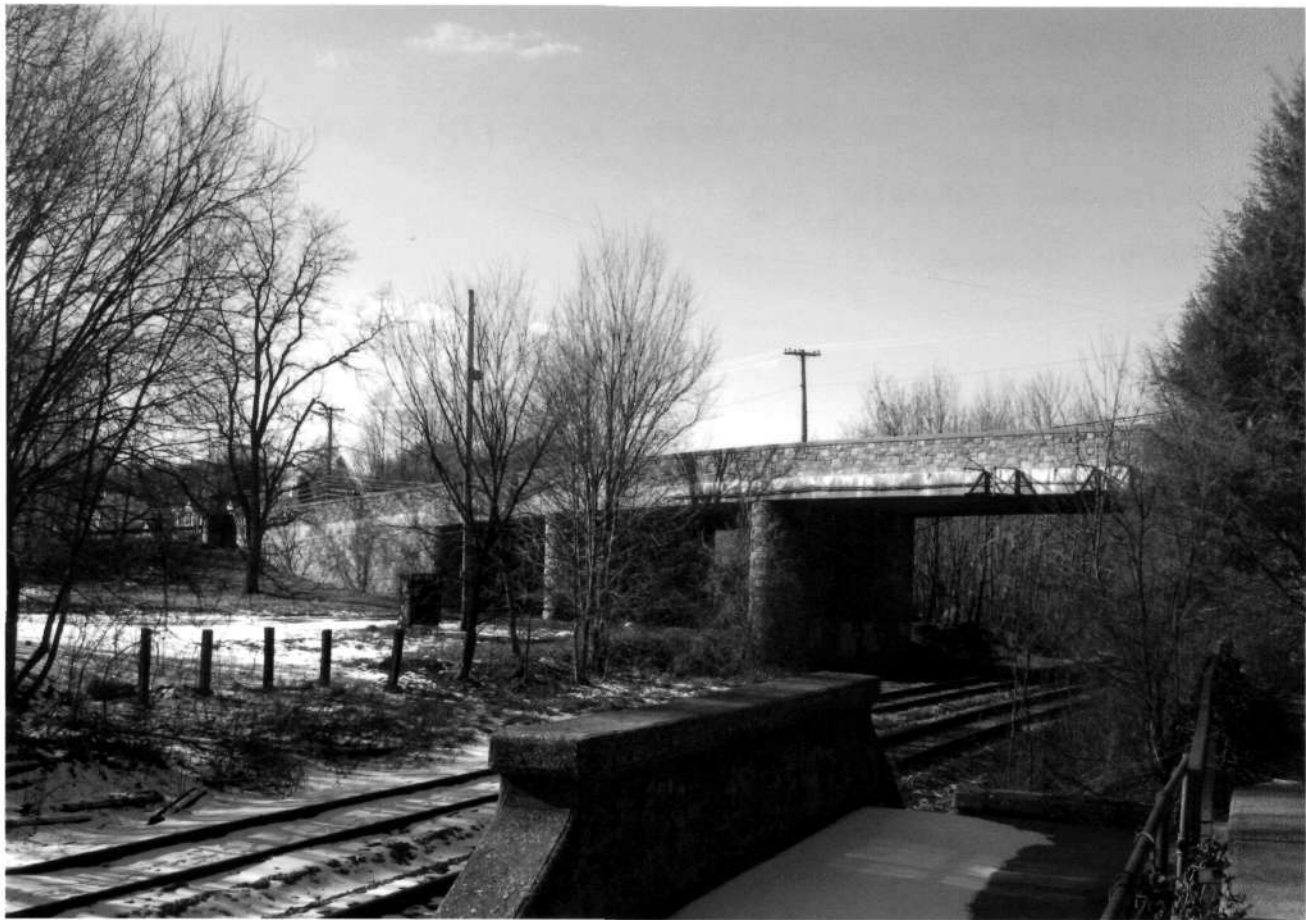
MIHP No. BA-2070
SHA Bridge No. 0307100
MD 128 Over WMRR
Baltimore County, Maryland

Photograph Log

Image File Name	Description of View
BA-2070_2009-02-06_01.tif	Southern elevation, facing northwest
BA-2070_2009-02-06_02.tif	Northern elevation, facing southwest
BA-2070_2009-02-06_03.tif	Southern elevation showing spandrel, parapet, and pier, facing north
BA-2070_2009-02-06_04.tif	Northern parapet wall facing southeast
BA-2070_2009-02-06_05.tif	Inner northern parapet wall and endpost with decorative urn, facing north
BA-2070_2009-02-06_06.tif	Efflorescence and staining of underside of deck, facing west
BA-2070_2009-02-06_07.tif	Pier and column of substructure, facing north
BA-2070_2009-02-06_08.tif	Western approach, facing northeast

Printed on Epson Premium Photo Paper Glossy with Epson UltraChrome Black Ink

Saved on Verbatim UltraLife Archival Grade DVD-R, AZO recording dye



MIHP BA-2070

SHA BRIDGE No. 0307100, MD 128 OVER WMRR

BALTIMORE COUNTY, MD

JAMES SKOICK

2/6/09

MD SHPO

SOUTHERN ELEVATION, FACING NORTHWEST

#1 of 8



MIHP BA-2070

SHA BRIDGE NO. 0307100, MD 128 OVER WMRR
BALTIMORE COUNTY, MD

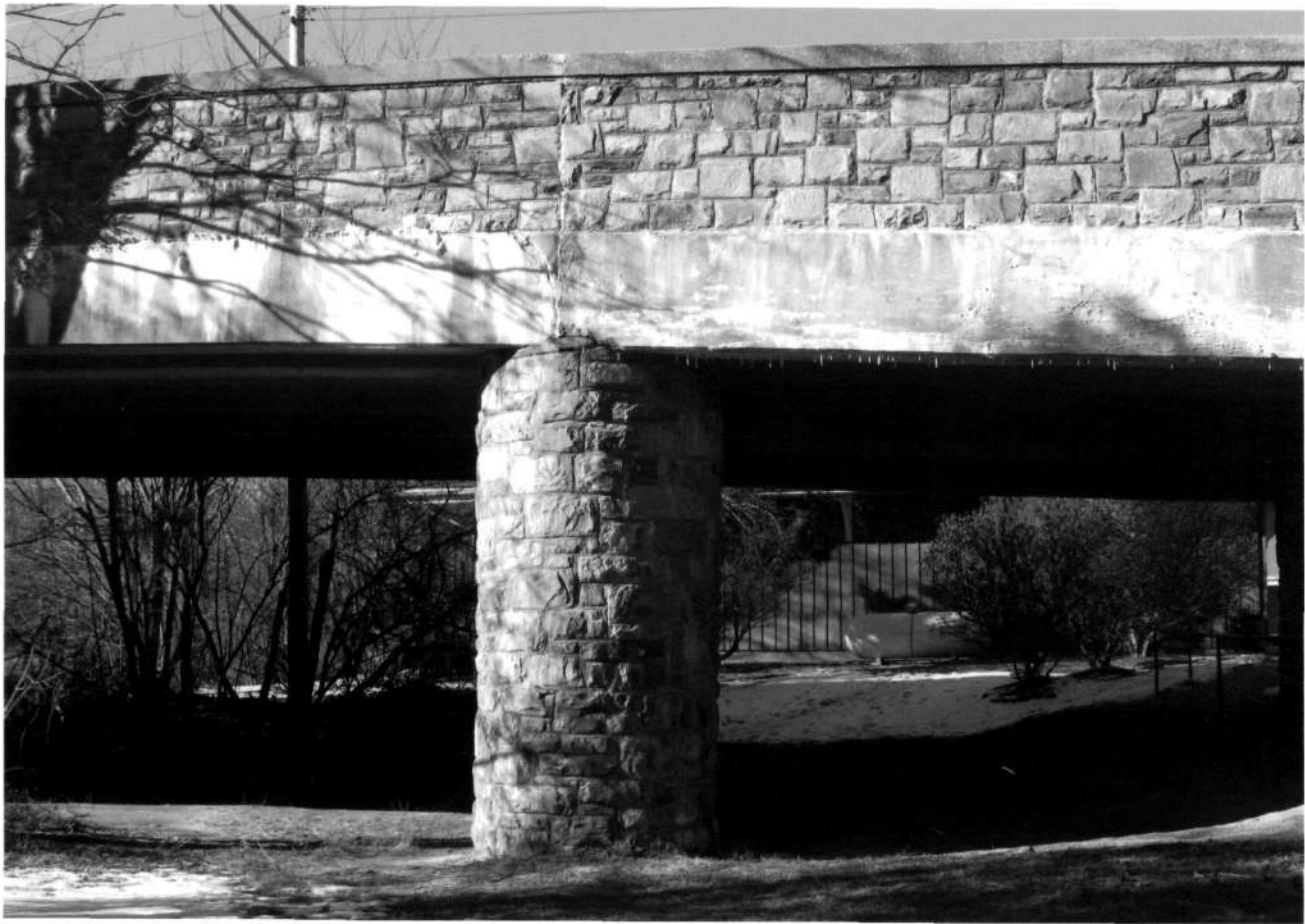
JAMES SKOCH

2/6/09

MD SHPO

NORTHERN ELEVATION, FACING SOUTHWEST

#2 of 8



MIHP BA-2070

SHA BRIDGE NO. 0307100, MD 128 OVER WMRR
BALTIMORE COUNTY, MD

JAMES SKOLIK

2/6/09

MD SHPO

SOUTHERN ELEVATION SHOWING SPANDREL, PARAPET,
AND ~~COLUMN~~ ^{PIER} FACING NORTH

3 OF 8



MIHP BA - 2070

SHA BRIDGE NO. 0307100, MD 128 OVER WMRR

BALTIMORE COUNTY, MD

JAMES SKOIK

2/6/09

MD SHPD

NORTHERN PARAPET WALL FACING SOUTHEAST

4 OF 8



MIHP BA -2070

SHA BRIDGE NO. 0307100, MD 128 OVER WMRR

BALTIMORE COUNTY, MD

JAMES SKOCIK

2/6/09

MD SHPD

INNER NORTHERN PARAPET WALL AND ENDPOST WITH
DECORATIVE URN, FACING NORTH

#5 of 8



MHP BA - 2070

SHA BRIDGE NO 0307100, MD 128 OVER WMRR
BALTIMORE COUNTY, MD

JAMES SKOCIK

2/6/09

MD SHPD

EFFLORESCENCE AND STAINING OF UNDERSIDE
OF DECK

6 of 8



MIHP BA - 2070

SHA BRIDGE NO. 0301700, MD 128 OVER WMRR
BALTIMORE COUNTY, MD

JAMES SKOCIK

2/6/09

MD SHPO

PIER AND COLUMN OF SUBSTRUCTURE, FACING NORTH

7 of 8



MIHP BA-2070

SHA BRIDGE NO. 030710D, MD 128 OVER WMRR
BALTIMORE COUNTY, MD

JAMES SKOICK

2/6/09

MD SHPO

WESTERN APPROACH, FACING NORTHEAST

8 of 8

Maryland Historical Trust

Maryland Inventory of Historic Properties number: BA-2070

Name: 3071/MD128 over Western Md. Railroad

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridge received the following determination of eligibility.

MARYLAND HISTORICAL TRUST	
Eligibility Recommended <u>X</u>	Eligibility Not Recommended _____
Criteria: <u> </u> A <u> </u> B <u> </u> C <u> </u> D Considerations: <u> </u> A <u> </u> B <u> </u> C <u> </u> D <u> </u> E <u> </u> F <u> </u> G <u> </u> None	
Comments: _____	

Reviewer, OPS: <u>Anne E. Bruder</u>	Date: <u>3 April 2001</u>
Reviewer, NR Program: <u>Peter E. Kurtze</u>	Date: <u>3 April 2001</u>

MARYLAND INVENTORY OF HISTORIC BRIDGES
HISTORIC BRIDGE INVENTORY
MARYLAND STATE HIGHWAY ADMINISTRATION/
MARYLAND HISTORICAL TRUST

MHT No. BA-2070

SHA Bridge No. 3071 Bridge name MD 128 over Western Maryland Railroad

LOCATION:

Street/Road name and number [facility carried] MD 128 (Butler Road)

City/town Glyndon

Vicinity _____

County Baltimore

This bridge projects over: Road _____ Railway X Water _____ Land _____

Ownership: State X County _____ Municipal _____ Other _____

HISTORIC STATUS:

Is the bridge located within a designated historic district? Yes X No _____

National Register-listed district _____ National Register-determined-eligible district _____

Locally-designated district _____ Other _____

Name of district Glyndon Historic District

BRIDGE TYPE:

Timber Bridge _____:

Beam Bridge _____ Truss -Covered _____ Trestle _____ Timber-And-Concrete _____

Stone Arch Bridge _____

Metal Truss Bridge _____

Movable Bridge _____:

Swing _____

Vertical Lift _____

Bascule Single Leaf _____

Retractable _____

Bascule Multiple Leaf _____

Pontoon _____

Metal Girder _____:

Rolled Girder _____

Plate Girder _____

Rolled Girder Concrete Encased _____

Plate Girder Concrete Encased _____

Metal Suspension _____

Metal Arch _____

Metal Cantilever _____

Concrete X _____:

Concrete Arch _____ Concrete Slab X Concrete Beam _____ Rigid Frame _____

Other _____ Type Name _____

DESCRIPTION:

Setting: Urban _____ Small town X Rural _____

Describe Setting:

Bridge No. 3071 carries MD 128 (Butler Road) over the Western Maryland Railroad in Baltimore County. MD 128 runs east-west and the Western Maryland Railroad runs north-south. The bridge is located in the town of Glyndon and is surrounded by open space, a railroad station constructed in 1879, and late-nineteenth and early-twentieth century single family dwellings.

Describe Superstructure and Substructure:

Bridge No. 3071 is a 5-span, 2-lane, concrete slab bridge. The bridge was originally built in 1947. The structure is 214 feet long and has a clear roadway width of 30 feet; there are two (2) sidewalks each measuring 4 feet wide. The out-to-out width is 40 feet. The concrete slab measures 2 feet, 8 inches thick and it has a bituminous wearing surface. The structure has solid concrete parapets with stone veneer and a concrete coping. The parapet endposts are topped with decorative urns. The roadway approaches slope up to the bridge. A date plaque on the north parapet reads as follows:

Glyndon Bridge
Built-1947

Designed by: George W.M. Stephens and Associates and Palmer and Lamdin
Constructed by: Allied Contractors, Inc.

The substructure consists of two (2) concrete abutments with stone veneer and four (4) concrete piers with stone veneer and concrete caps. The bridge is posted for 27.5 tons and has a sufficiency rating of 87.7.

According to the 1996 inspection report, this structure is in satisfactory condition with structural elements showing only minor deterioration. The asphalt wearing surface has recently been replaced and is in good condition. The concrete has cracking in the slab, abutments, and mortar joints of the piers. The piers also have loose stones. Also, the concrete parapets have loose stones and cracking in the concrete coping.

Discuss Major Alterations:

Inspection reports from 1996 indicate that the deck was overlaid and the joints modified in 1994.

HISTORY:

WHEN was the bridge built: 1947

This date is: Actual X Estimated _____

Source of date: Plaque X Design plans _____ County bridge files/inspection form _____

Other (specify): State Highway Administration bridge files/inspection form

WHY was the bridge built?

The bridge was constructed in response to the need for a more efficient transportation network and increased load capacity.

WHO was the designer?

According to the plaque on the bridge, the structure was designed by George W.M. Stephens and Associates and Palmer and Lamdin.

WHO was the builder?

According to the plaque on the bridge, the structure was built by Allied Contractors, Inc.

WHY was the bridge altered?

N/A

Was this bridge built as part of an organized bridge-building campaign?

There is no evidence that the bridge was built as part of an organized bridge building campaign.

SURVEYOR/HISTORIAN ANALYSIS:

This bridge may have National Register significance for its association with:

A - Events _____ B- Person _____
C- Engineering/architectural character X

The bridge is located in the Glyndon Historic District, which is listed on the National Register of Historic Places. Although the bridge appears to post-date the district's period of significance, the structure does not visually detract from the appearance of the district. Furthermore, the bridge is individually eligible for the National Register of Historic Places under Criterion C, as a significant example of concrete slab bridge construction. The structure has a high degree of integrity, retaining such character-defining elements of the type as the slab, parapets, abutments, and piers, and is a stylized combination of concrete and masonry veneer construction in the late 1940s. Palmer and Lamdin, known society architects, were responsible for the Late Art Deco or early Moderne design of the bridge.

Was the bridge constructed in response to significant events in Maryland or local history?

Reinforced concrete slab bridges are a twentieth century structure type, easily adapted to the need for expedient engineering solutions. Reinforced concrete technology developed rapidly in the early twentieth century with early recognition of the potential for standardized design. The first U.S. attempt to standardize concrete design specifications came in 1903-1904 with the formation of the Joint Committee on Concrete and Reinforced Concrete of the American Society of Civil Engineers.

Maryland's roads and bridge improvement programs mirrored economic cycles. The first road improvement of the State Roads Commission was a 7 year program, starting with the Commission's establishment in 1908 and ending in 1915. Due to World War I, the period from 1916-1920 was one of relative inactivity; only roads of first priority were built. Truck traffic resulting from war related factories and military installations generated new, heavy traffic unanticipated by the builders of the early road system. From 1920-1929, numerous highway improvements occurred in response to the increase in Maryland motor vehicles from 103,000 in 1920 to 320,000 in 1929, with emphasis on the secondary system of feeder roads which moved traffic from the primary roads built before World War I. After World War I, Maryland's bridge system also was appraised as too narrow and structurally inadequate for the increasing traffic, with plans for an expanded bridge program to be

handled by the Bridge Division, set up in 1920. In 1920 under Chapter 508 of the Acts of 1920 the State issued a bond of \$3,000,000.00 for road construction; the primary purpose of these monies was to meet the state obligations involving the construction of rural post roads. The secondary purpose of these monies was to fund (with an equal sum from the counties) the building of lateral roads. The number of hard surfaced roads on the state system grew from 2000 in 1920 to 3200 in 1930. By 1930, Maryland's primary system had been inadequate to the huge freight trucks and volume of passenger cars in use, with major improvements occurring in the late 1930's. Most improvements to local roads waited until the years after World War I.

In the early years, there was a need to replace the numerous single lane timber bridges. Walter Wilson Crosby, Chief Engineer, stated in 1906, "the general plan has been to replace these [wood bridges] with pipe culverts or concrete bridges and thus forever do away with the further expense of the maintenance of expensive and dangerous wooden structures." Within a few years, readily constructed standardized bridges of concrete were being built throughout the state.

In 1930, the roadway width for all standard plan bridges was increased to 27 feet in order to accommodate the increasing demands of automobile and truck traffic (State Roads Commission 1930). The range of span lengths remained the same, but there were some changes designed to increase the load bearing capacities. The reinforcing bars increased in thickness. Visually, the 1930 design can be distinguished from its predecessors by the pierced concrete railing that was introduced at this time.

In 1933, a new set of standard plans were introduced by the State Roads Commission. This time their preparation was not announced in the Report; new standard plans were by this time nothing special - they had indeed become standard. Once again accommodating the ever-increasing demands of traffic, the roadway was increased, this time to 30 feet. The slab span's reinforcing bars remained the same diameter but were placed closer together to achieve still more load capacity.

When the bridge was built and/or given a major alteration, did it have a significant impact on the growth and development of the area?

There is no evidence that the construction of this bridge had a significant impact on the growth and development of this area.

Is the bridge located in an area which may be eligible for historic designation and would the bridge add to or detract from the historic/visual character of the potential district?

This bridge is located within the Glyndon Historic District, which is listed on the National Register of Historic Places. According to the National Register nomination form, the district is significant as a turn of the century community. Although the bridge was constructed in 1947 and therefore appears to post-date the period of significance, the bridge does not visually detract from the historic appearance of the district.

Is the bridge a significant example of its type?

The bridge is a potentially significant example of a concrete slab bridge, possessing distinctive ornamentation and design. Bridge 3071 possesses a distinct style and design for a concrete slab bridge. It is a good example of a bridge with Moderne or Late Art Deco influences. According to the Maryland Historic Bridge Inventory, the Glyndon Bridge is one of the few structures of its type to have benefitted from the design input of an architect, the firm of Palmer and Lamdin.

Does the bridge retain integrity of important elements described in Context Addendum?

The bridge retains the character-defining elements of its type, as defined by the Statewide Historic Bridge Context, including the original slab, parapets, abutments, and piers.

Is the bridge a significant example of the work of a manufacturer, designer, and/or engineer?

This bridge is a significant example of the work of George W.M. Stephens and Associates and Palmer and Lamdin.

Should the bridge be given further study before an evaluation of its significance is made?

No further study of this bridge is required to evaluate its significance.

BIBLIOGRAPHY:

County inspection/bridge files _____ SHA inspection/bridge files X
Other (list):

Ketchum, Milo S.

1908 *The Design of Highway Bridges and the Calculation of Stresses in Bridge Trusses.* The Engineering News Publishing Co., New York.

1920 *The Design of Highway Bridges of Steel, Timber and Concrete.* Second edition. McGraw-Hill Book Company, New York.

Lay, Maxwell Gordon

1992 *Ways of the World: A History of the World's Roads and of the Vehicles That Used Them.* Rutgers University Press, New Brunswick, New Jersey.

Maryland State Highway Administration

Draft of Maryland Historic Bridges Inventory. State of Maryland, State Highway Administration, Baltimore, 1991.

Maryland State Roads Commission

1930a *Report of the State Roads Commission for the Years 1927, 1928, 1929 and 1930.* State of Maryland, State Roads Commission, Baltimore.

1930b *Standard Plans.* State of Maryland, State Roads Commission, Baltimore.

Taylor, Frederick W., Sanford E. Thompson, and Edward Smulski

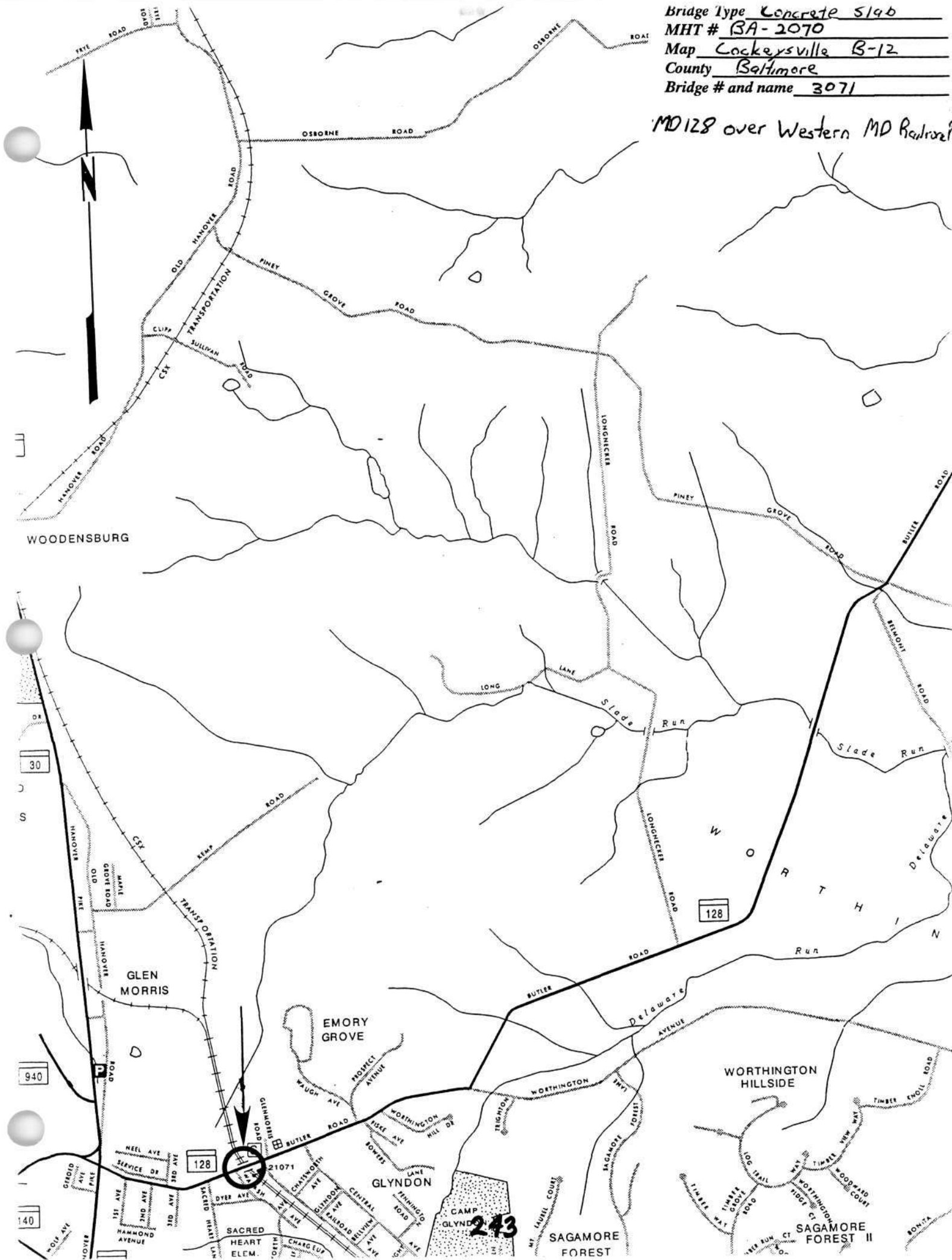
1939 *Reinforced-Concrete Bridges with Formulas Applicable to Structural Steel and Concrete.* John Wiley & Sons, Inc., New York.

Tyrrell, H. Grattan

1909 *Concrete Bridges and Culverts for Both Railroads and Highways.* The Myron C. Clark Publishing Company, Chicago and New York.

Bridge Type Concrete 514b
MHT # BA-2070
Map Cockeysville B-12
County Baltimore
Bridge # and name 3071

MD 128 over Western MD Railroad?





1. BA-2570
2. MD 128 over Western MD Road
3. Baltimore County (3071)
4. Eric Shiffitts
5. 3/97
6. MD SHPO
7. south elevation
8. 1 of 6



1. BA 2070
2. MD 128 over western MD Road
3. Baltimore County (3071)
4. Eric Griffiths
5. 3/97
6. MD SHPD
7. last approach
8. 2 of 6



1. BA-2070
2. MD 128 over western MD road
3. Baltimore County (3071)
4. Eric Gruffitts
5. 3/97
6. MD SHPD
7. west approach
8. 3 of 6



1. BA 2075
2. MD 128 over western MD Road
3. B. Timmer Crainby (3071)
4. Eric Gruffitts
5. 3/97
6. MD SHPO
7. north elevation
8. 4 of 6



1. BA 2070
2. MD 100 near Western MD Road
3. Baltimore County (2011)
4. Eric Shuffitts
5. 3/97
6. MD SHPO
7. detail of slab + pier
8. 5 of 6



1. BA-2070
2. MD 128 over Western MD Road
3. Baltimore County (307)
4. Eric Gruffitts
5. 3/97
6. MD SHPO
7. detail of east abutment
8. 6 of 6

9100 448

**CONTRIBUTING RESOURCE
MARYLAND HISTORICAL TRUST
INTERNAL NR-ELIGIBILITY REVIEW FORM**

Property Name: Bridge #3071 Survey Number: BA 2070

Property Address: MD 128 over Western Maryland RR, Glyndon, Baltimore
County

Project: Repair of Bridge #3071 Agency: FSHA

Site visit by MHT Staff: X no yes Name Date

District Name: Glyndon Historic District Survey Number: BA 2210

X Listed Eligible Comment

Criteria: A B X C D Considerations: A B C D E F X G None

The resource X contributes/ does not contribute to the historic significance of this historic district in:

X Location X Design X Setting Materials

X Workmanship Feeling X Association

Justification for decision: (Use continuation sheet if necessary and attach map)

Bridge #3071 is a notable feature in the Glyndon Historic District. Constructed c. 1947 the bridge is a fine example of the Moderne style. Typically concrete bridges constructed in Maryland from the 1930s on used undisguised construction techniques. The Glyndon Bridge is sheathed in stone and reflects Moderne design influences. These influences are particularly obvious in the large stylized concrete urns at each terminus of the flared parapet walls. The bridge, constructed by the State of Maryland, is unusual in having been designed by a team which included architects, the firm of Palmer and Lamdin.

Although outside the period of significance for the Glyndon Historic District, the bridge contributes to the district through its fine design. Moreover, the unusual attention to design suggests a recognition by the state of the special aesthetic qualities of Glyndon. The residential community is characterized by a variety of architectural styles of the late 19th century through the early 20th century. Glyndon's early growth was spurred by the arrival of the railroad in 1860. With the rise of the automobile, the summer resort became a full-time community. The bridge, which carries Maryland Route 128 over the Western Railroad right-of-way, reflects the confluence of the two modes of transportation which shaped Glyndon.

Documentation on the property is presented in: Inventory Form BA 2070

Prepared by: John Hnedak, augmented by Rita Suffness

Elizabeth Hannold

Reviewer, Office of Preservation Services

1/2/92

Date

NR program concurrence: X yes no not applicable

R. Andrews
Reviewer, NR program

6 Jan 92
Date

Jan 92

MARYLAND COMPREHENSIVE HISTORIC PRESERVATION PLAN DATA - HISTORIC CONTEXT

I. Geographic Region:

- ☐ Eastern Shore (all Eastern Shore counties, and Cecil)
☐ Western Shore (Anne Arundel, Calvert, Charles, Prince George's and St. Mary's)
☒ Piedmont (Baltimore City, Baltimore, Carroll, Frederick, Harford, Howard, Montgomery)
☐ Western Maryland (Allegany, Garrett and Washington)

II. Chronological/Developmental Periods:

- | | |
|---|---------------------|
| <input type="checkbox"/> Paleo-Indian | 10000-7500 B.C. |
| <input type="checkbox"/> Early Archaic | 7500-6000 B.C. |
| <input type="checkbox"/> Middle Archaic | 6000-4000 B.C. |
| <input type="checkbox"/> Late Archaic | 4000-2000 B.C. |
| <input type="checkbox"/> Early Woodland | 2000-500 B.C. |
| <input type="checkbox"/> Middle Woodland | 500 B.C. - A.D. 900 |
| <input type="checkbox"/> Late Woodland/Archaic | A.D. 900-1600 |
| <input type="checkbox"/> Contact and Settlement | A.D. 1570-1750 |
| <input type="checkbox"/> Rural Agrarian Intensification | A.D. 1680-1815 |
| <input type="checkbox"/> Agricultural-Industrial Transition | A.D. 1815-1870 |
| <input checked="" type="checkbox"/> Industrial/Urban Dominance | A.D. 1870-1930 |
| <input type="checkbox"/> Modern Period | A.D. 1930-Present |
| <input type="checkbox"/> Unknown Period (<input type="checkbox"/> prehistoric <input type="checkbox"/> historic) | |

III. Prehistoric Period Themes:

- ☐ Subsistence
☐ Settlement

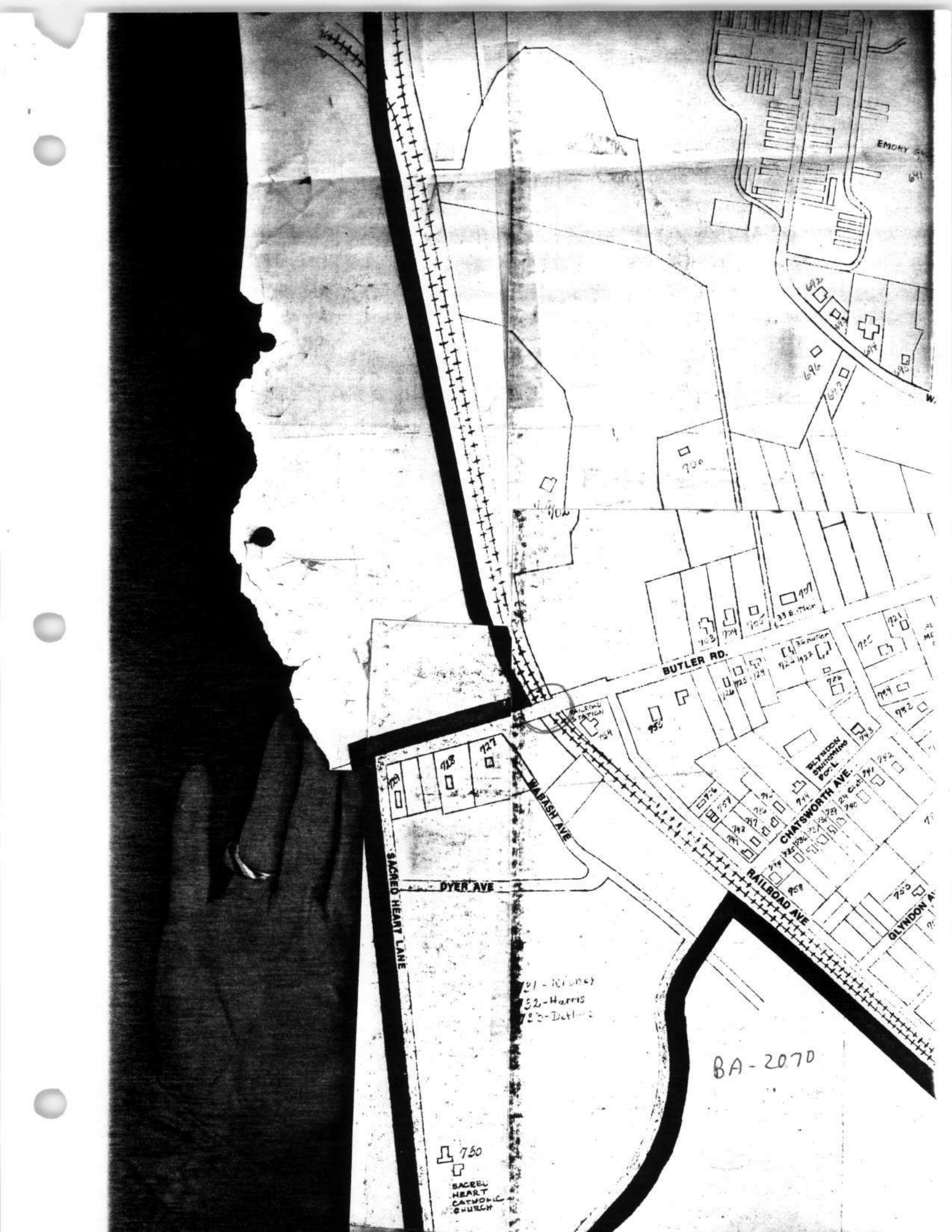
☐ Political
☐ Demographic
☐ Religion
☐ Technology
☐ Environmental Adaption

IV. Historic Period Themes:

- ☐ Agriculture
☒ Architecture, Landscape Architecture, and Community Planning
☐ Economic (Commercial and Industrial)
☐ Government/Law
☐ Military
☐ Religion
☐ Social/Educational/Cultural
☐ Transportation

V. Resource Type:

Category: structureHistoric Environment: suburbanHistoric Function(s) and Use(s): transportationKnown Design Source: Palmer and Lambdin



EMORY ST

BUTLER RD.

CHATSWORTH AVE

RAILROAD AVE

SACRED HEART LANE

DYER AVE

750
SACRED HEART CATHOLIC CHURCH

71 - KIRBY
32 - HARRIS
750 - DETMERS

BA-2070

BA-2070

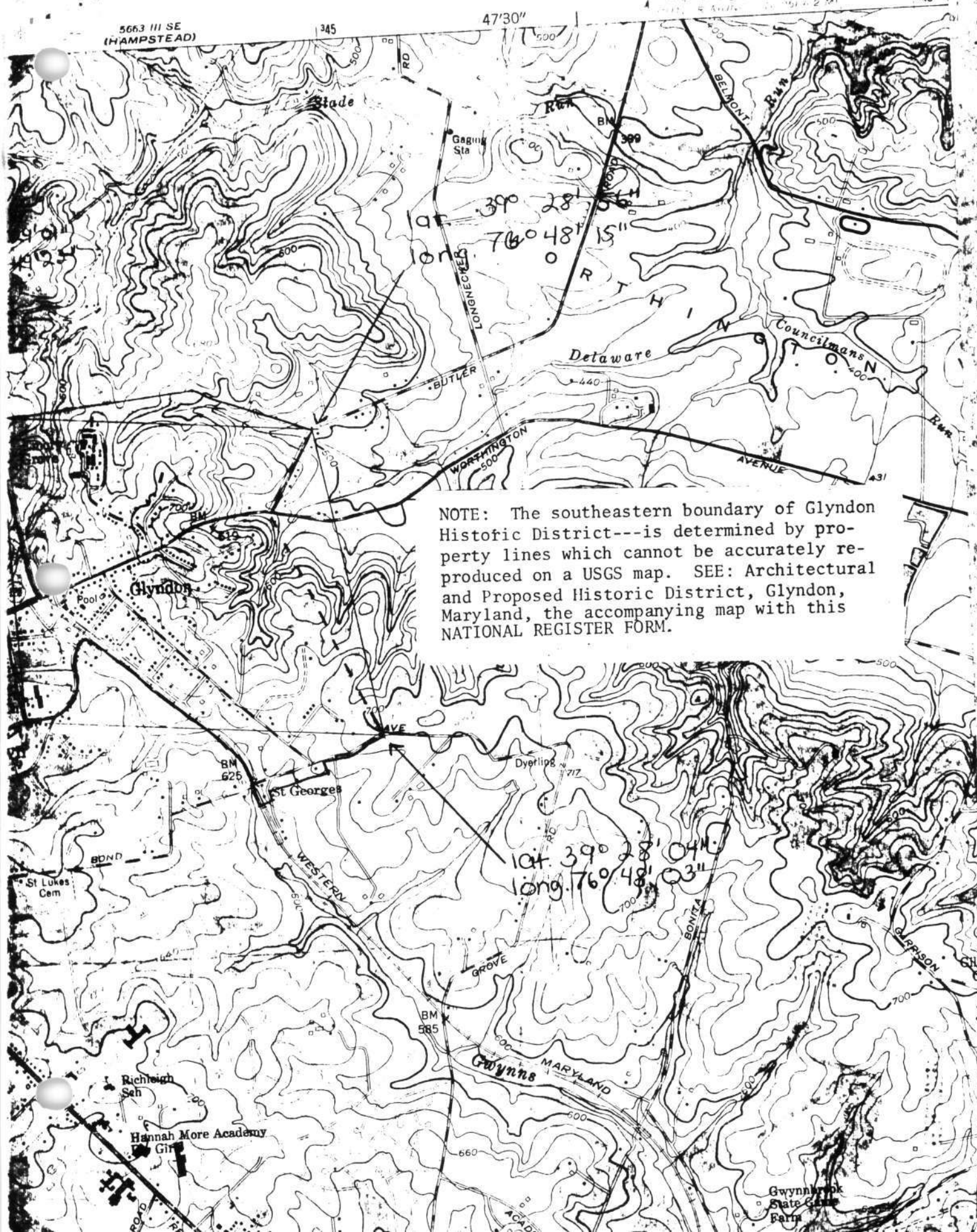
7.5 MINI
NE

348

5663 III SE
(HAMPSTEAD)

345

47°30'



NOTE: The southeastern boundary of Glyndon Historic District---is determined by property lines which cannot be accurately reproduced on a USGS map. SEE: Architectural and Proposed Historic District, Glyndon, Maryland, the accompanying map with this NATIONAL REGISTER FORM.

lat 39° 28' 04"
long 76° 48' 03"

BA-2070
Glyndon Bridge
1949
public

The Glyndon. Bridge is a very rare example of stylishness in bridge design, one of the few such structures to have benefitted from the design input of an architect, the firm of Palmer and Lamdin. A very fine example of Moderne or late Art Deco style.

MAGI # 0320703817

INVENTORY FORM FOR STATE HISTORIC SITES SURVEY

1 NAME

HISTORIC

AND/OR COMMON

Glyndon Bridge

2 LOCATION

STREET & NUMBER

Maryland Route 128 over Western Maryland Railroad right-of-way

CITY, TOWN

Glyndon

VICINITY OF

CONGRESSIONAL DISTRICT

2nd

STATE

Maryland

COUNTY

Baltimore

3 CLASSIFICATION

CATEGORY

☐ DISTRICT☐ BUILDING(S)☒ STRUCTURE☐ SITE☐ OBJECT

OWNERSHIP

☒ PUBLIC☐ PRIVATE☐ BOTH

PUBLIC ACQUISITION

☐ IN PROCESS☐ BEING CONSIDERED

STATUS

☒ OCCUPIED☐ UNOCCUPIED☐ WORK IN PROGRESS

ACCESSIBLE

☐ YES: RESTRICTED☒ YES: UNRESTRICTED☐ NO

PRESENT USE

☐ AGRICULTURE☐ COMMERCIAL☐ EDUCATIONAL☐ ENTERTAINMENT☐ GOVERNMENT☐ INDUSTRIAL☐ MILITARY☐ MUSEUM☐ PARK☐ PRIVATE RESIDENCE☐ RELIGIOUS☐ SCIENTIFIC☒ TRANSPORTATION☐ OTHER:**4 OWNER OF PROPERTY**

NAME

Maryland Department of Transportation
State Highway Administration

Telephone #:

STREET & NUMBER

CITY, TOWN

VICINITY OF

STATE, zip code

5 LOCATION OF LEGAL DESCRIPTIONCOURTHOUSE,
REGISTRY OF DEEDS, ETC.

Baltimore County Court House

Liber #:

Folio #:

STREET & NUMBER

CITY, TOWN

Towson

MD

STATE

6 REPRESENTATION IN EXISTING SURVEYS

TITLE

DATE

☐ FEDERAL ☐ STATE ☐ COUNTY ☐ LOCALDEPOSITORY FOR
SURVEY RECORDS

CITY, TOWN

STATE

7 DESCRIPTION BA-2070**CONDITION**

☒ EXCELLENT
☐ GOOD
☐ FAIR

☐ DETERIORATED
☐ RUINS
☐ UNEXPOSED

CHECK ONE

☒ UNALTERED
☐ ALTERED

CHECK ONE

☒ ORIGINAL SITE
☐ MOVED DATE _____

DESCRIBE THE PRESENT AND ORIGINAL (IF KNOWN) PHYSICAL APPEARANCE

The Glyndon Bridge, which carries Maryland Route 128 over the Western Maryland Railroad right-of-way just west of the town of Glyndon in Baltimore County, is a 380' span of concrete with a coursed rubble stone veneer. The two lane roadway is 41' wide including a narrow medium strip and sidewalks on the outside. The stone parapets which flank the roadway are finished with a coping of concrete. They terminate by flaring slightly away from the center of the road, curving back, scroll-like, to provide for four large concrete urns. The urns are decidedly "modern", as is the steel guard rail which extends from the N-E parapet. The urns are lighted from beneath by concealed lamps, an effect which was highly regarded by practitioners of the modern style. These decorative details are responsible for the general "deco" appearance of the structure.

RECEIVED BY _____

CONTINUE ON SEPARATE SHEET IF NECESSARY

PERIOD		AREAS OF SIGNIFICANCE -- CHECK AND JUSTIFY BELOW				
<input type="checkbox"/> PREHISTORIC	<input type="checkbox"/> ARCHEOLOGY-PREHISTORIC	<input type="checkbox"/> COMMUNITY PLANNING	<input type="checkbox"/> LANDSCAPE ARCHITECTURE	<input type="checkbox"/> RELIGION		
<input type="checkbox"/> 1400-1499	<input type="checkbox"/> ARCHEOLOGY-HISTORIC	<input type="checkbox"/> CONSERVATION	<input type="checkbox"/> LAW	<input type="checkbox"/> SCIENCE		
<input type="checkbox"/> 1500-1599	<input type="checkbox"/> AGRICULTURE	<input type="checkbox"/> ECONOMICS	<input type="checkbox"/> LITERATURE	<input type="checkbox"/> SCULPTURE		
<input type="checkbox"/> 1600-1699	<input checked="" type="checkbox"/> ARCHITECTURE	<input type="checkbox"/> EDUCATION	<input type="checkbox"/> MILITARY	<input type="checkbox"/> SOCIAL/HUMANITARIAN		
<input type="checkbox"/> 1700-1799	<input type="checkbox"/> ART	<input checked="" type="checkbox"/> ENGINEERING	<input type="checkbox"/> MUSIC	<input type="checkbox"/> THEATER		
<input type="checkbox"/> 1800-1899	<input type="checkbox"/> COMMERCE	<input type="checkbox"/> EXPLORATION/SETTLEMENT	<input type="checkbox"/> PHILOSOPHY	<input checked="" type="checkbox"/> TRANSPORTATION		
<input checked="" type="checkbox"/> 1900-	<input type="checkbox"/> COMMUNICATIONS	<input type="checkbox"/> INDUSTRY	<input type="checkbox"/> POLITICS/GOVERNMENT	<input type="checkbox"/> OTHER (SPECIFY)		
		<input type="checkbox"/> INVENTION				

SPECIFIC DATES 1942 BUILDER/ARCHITECT Palmer & Lambdin, Architects
 STATEMENT OF SIGNIFICANCE

The Glyndon bridge is most notable for its stylishness, a rare characteristic of bridge design. The structure may be classified as "modern", a term which refers to a late (1930s and 1940s) development of the Art Deco style. The style's most exuberant expressions are to be found in bus terminals, train stations and even gas stations. The expression of forward motion was the essence of the style, found in the use of horizontal elements dominating rhythmically spaced verticals, as in the guard rails of the Glyndon bridge, and of "aerodynamic forms" like the rounded parapets. The style is well suited to transportation related structures.

Most bridges built by the state of Maryland in the 30s and 40s used undisguised concrete construction techniques, and bridges like the one at Glyndon are extremely rare. Besides the stylistic devices mentioned earlier, the fact of its being a concrete structure sheathed in non-essential stone suggests an uncommon aesthetic striving. This is not inconsistent with the upper middle class tone of the town of Glyndon.

The bridge is unusual in having been designed by a team which included architects, the firm of Palmer and Lambdin.

9 MAJOR BIBLIOGRAPHICAL REFERENCES

see continuation sheet.

CONTINUE ON SEPARATE SHEET IF NECESSARY

10 GEOGRAPHICAL DATA

ACREAGE OF NOMINATED PROPERTY _____

Quadrangle Name: Reisterstown

Quadrangle Scale: 1:24 000

UTM References:

18.343420.4370900

VERBAL BOUNDARY DESCRIPTION

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE

COUNTY

STATE

COUNTY

11 FORM PREPARED BY

NAME / TITLE

John Hnedak/M/DOT Survey Manager

ORGANIZATION

Maryland Historical Trust

STREET & NUMBER

21 State Circle

CITY OR TOWN

Annapolis

DATE

1980

TELEPHONE

(301) 269-2438

STATE

Maryland 21401

The Maryland Historic Sites Inventory was officially created by an Act of the Maryland Legislature, to be found in the Annotated Code of Maryland, Article 41, Section 181 KA, 1974 Supplement.

The Survey and Inventory are being prepared for information and record purposes only and do not constitute any infringement of individual property rights.

RETURN TO: Maryland Historical Trust
The Shaw House, 21 State Circle
Annapolis, Maryland 21401
(301) 267-1438

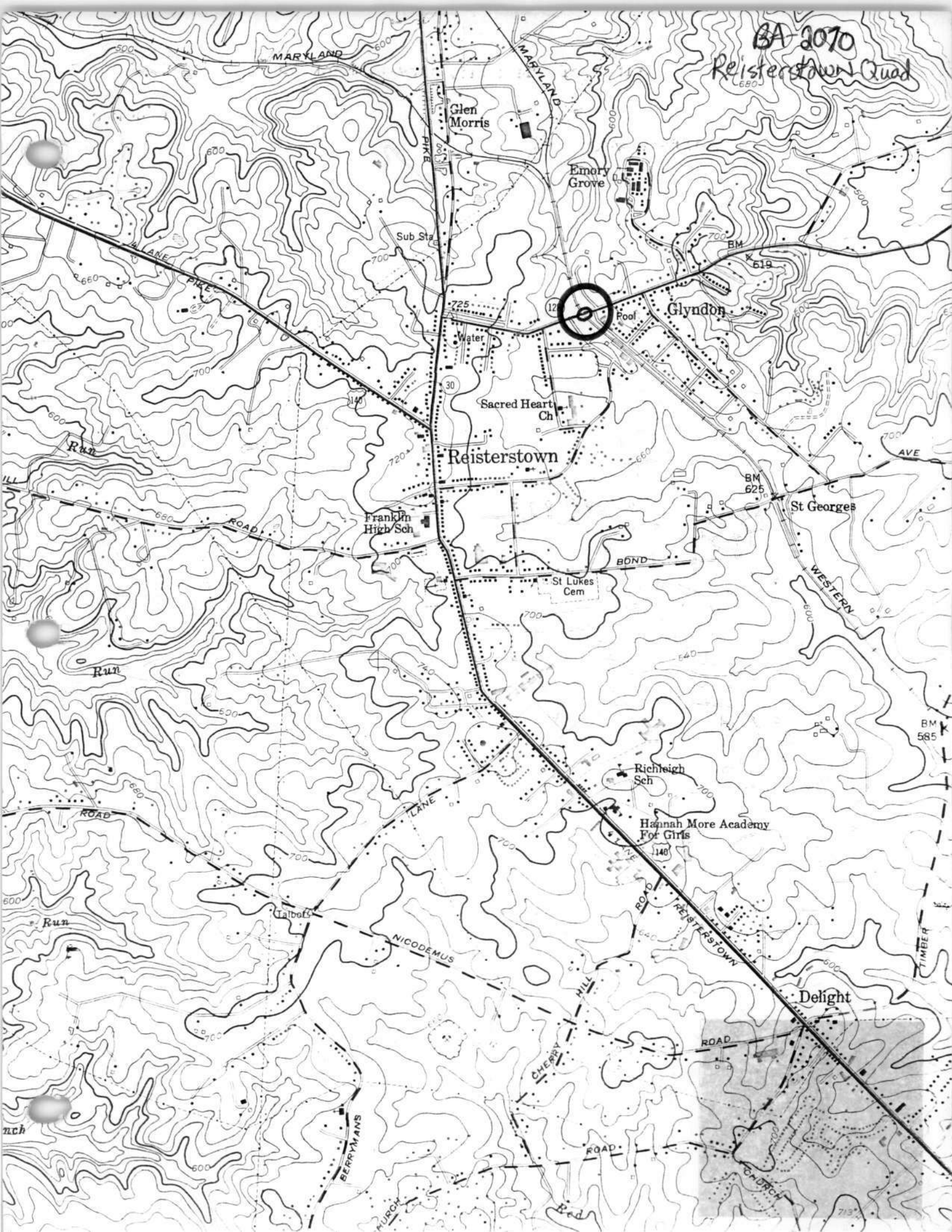
BA-2070
Glyndon Bridge

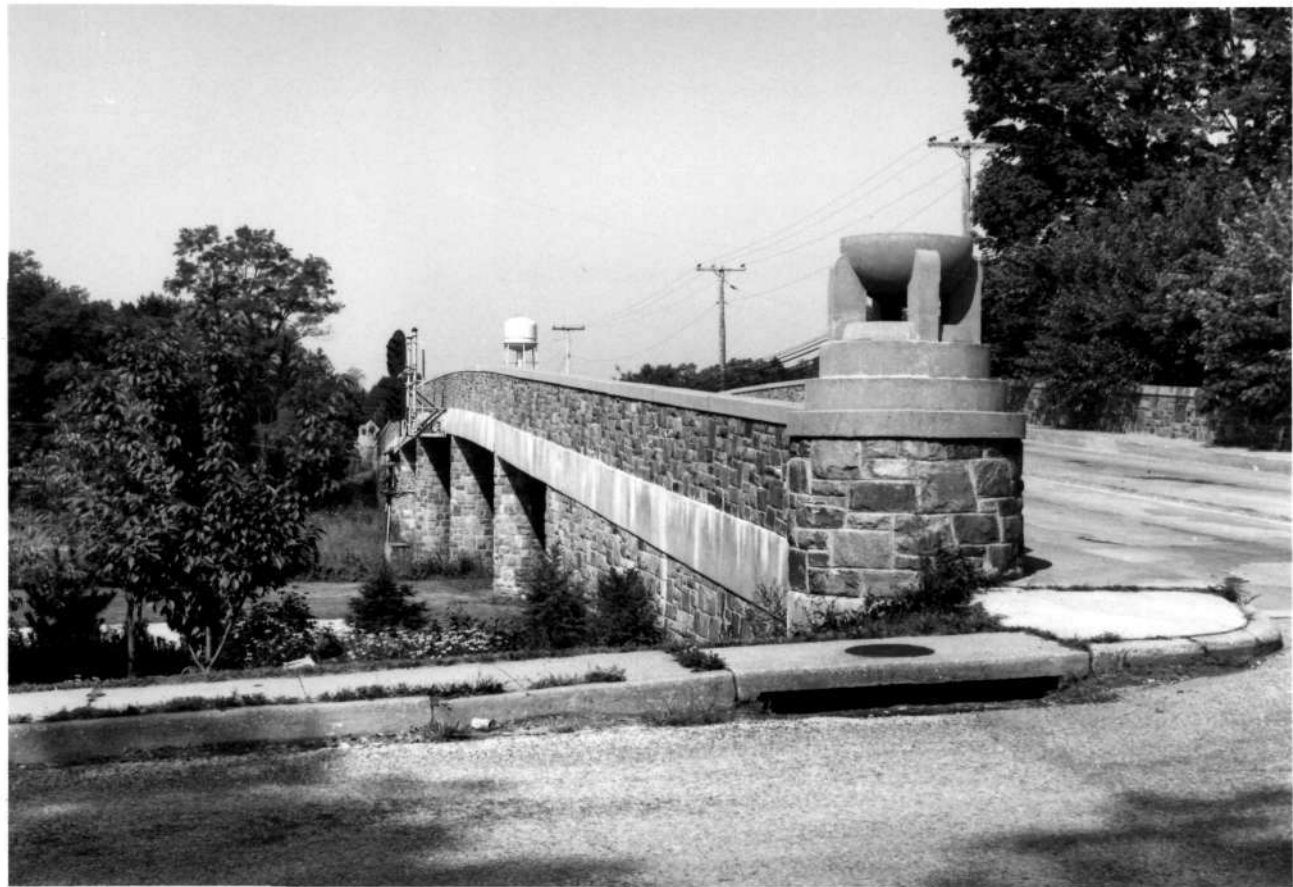
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BA 2070
Reisterstown Quad





BA-2070
Glyndon Bridge
M/DOT
Hnedak/Meyer
Summer ~~1978~~ 1980